



# The Mail Car

Newsletter of the St. Lawrence Division – NMRA

Issue no. 74 – May 2013

St. Lawrence Division web site: [www.sld-nmra.ca](http://www.sld-nmra.ca)

## *Superintendent's Report*

By Ron Newby

As I write this I have gotten back from the Streetsville Junction convention where I had a great time. There was a lot to see and do and the organizers did a couple of things differently. One of the great features was the Manufacturer's Showcase that they had on Saturday night where you could meet and talk directly to the manufacturers. Represented were Fast Tracks, ESU LokSound, Rapido, Sylvain Scale Models, Osborne Scale Models, Hunterline and Hamilton Model Works. As you can see there

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The four Amigos sporting the new SLD shirts at the SLD table at the Train Expo 2013  
Photo: Andreas Mank

## St. Lawrence Division

### Executive Officers

**Superintendent:** Ron Newby  
Phone (613) 841-4165  
Email: oapsry@magma.ca

**Assistant Superintendent:**  
Peter Gray  
Phone (613) 875-1335  
Email: peterg13@gmail.com

**Paymaster:** Debbie Stewart  
Phone (613) 761-1774  
Email:  
windermere630@rogers.com

**Clerk:** Gary Baillargeon  
Phone: (613) 774-2380  
Email: gary@petworthems.com

**Dispatcher:** Peter Nesbitt  
Phone (613) 592-0110  
Email: Nesbitt@magma.ca

### Appointed Positions

**Inspector:** Grant Knowles  
Phone (613) 825-5438  
Email: gd.knowles@sympatico.ca

**The Mail Car**  
Editor: Andreas Mank  
Phone: (613) 591-9088  
Email: amank@magma.ca

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was an impressive list of manufacturers represented. After that, there was a round robin discussion where it was interesting to hear what the manufacturer's issues were with producing their products and their take on the state of the hobby.

I would like to thank Peter Gray for all the work he put in getting the shirts and name tags done in record time. It was great seeing everyone wear them at the convention. If you did not order a shirt or name badge, it is not too late, just contact Peter.

The last meeting of the season will take place in Kingston on May 25th and we have another exciting line-up for you. We will be meeting at Bayridge Alliance Church and we will be presenting two clinics. The first one will be building a waterfront by Bob Farquhar, if you have never seen a clinic by Bob, you are in for a treat. The second clinic will be "Designing a Narrow Gauge Layout for Operations" by yours truly. I was supposed to do this clinic at the last meeting but John Stewart gave such a great clinic on working with metal where you got to use a lathe if you wanted to that we ran out of time. In the afternoon we have some great layouts to visit that we do not normally see.

On the Clearwater Valley Railway Co. front not much is happening on the layout as I am currently working out of town during the week and I am only home on weekends. I like to say that I had to get an apartment to save the marriage. One advantage has been that I can get some modeling done at nights as I brought my modeling tools with me and a couple of projects to work on so on that front it is working out great.

In the last **Mail Car** I mentioned that I was about 75% done on the HO scale Dallas Divide Section House that will be going in the corner. I am happy to report that it has been completed and in place. A couple of HO people sitting on a bench on the porch and a HO scale model A Ford complete the scene.

That is all I have for now and looking forward to seeing everyone in Kingston. Till then, may your trains stay on the track and may you never run out of rail.

# ***Streetsville Junction NFR Convention Report***

**By Stan Conley**

A sunny travel day for rail fanning, a few trains, and a few tail-ends, good facilities, interesting clinics, a variety of layout tours, meet and greet with model manufacturers and an interesting Q&A with the same, very late night shopping, great models, short functional AGM, a raffle with something for everyone, catch up with old friends, make some new ones, and a drive home on a warm summer day. I do not think you could ask for more from a convention!

The above pretty much summarizes the Streetsville Jct NFR convention held April 26-28, 2013 in Mississauga Ontario. The convention was well run with no serious missteps to speak of and everyone seemed to enjoy the weekend together sharing our communal interest.

I started the weekend off by gathering up my two travel companions John Stewart and Tom Badenoch for an early start with the promise of clearing skies and hopefully the possibility of catching some train action on the way west. First stop, John's restaurant in Brockville for breakfast, two westbound VIAs and one westbound freight viewed from our booth by the window.

Traveling west we scoot down to Brighton and manage to catch another couple of CN freights westbound. Then we travel west along Hwy 2 keeping watch on the main lines close by but we reach Cobourg before we see a train and we locate the VIA station in time to catch an eastbound passenger train. They are constructing a new VIA station with a high level pedestrian cross over and the CN employee there to warn the construction crews reveals that there will not be any freights for a while, so it is off to Port Hope. There we see why the traffic is so light (non-existent) on the CP, a rail inspection or grinder passes east just as we are about to pass under the viaduct, and when we are at the station; a CP Hi-rail follows him along. Just before we decide to press on a west bound CN with an Illinois Central unit roars through at the head of a container and autorack train.

We move on west, now following Lakeshore road as it keeps us within sight of the mains. We pull off Lakeshore when we spot a westbound CP and reach the old wooden bridge in time to see the tail end disappear around curve. At this point we have crossed the CN main and are about 400m north so we only see the eastbound freight as it passes the low section of the cut, and to add insult to injury, the tail of the westbound VIA is seen as rockets past on the south main! Welcome to rail fanning.

We hop back on the 401 at Newtonville and hi-tail it to Pickering for a sort stop at the Crossbuck Hobbies model train shop. From there a jaunt north to the 407 and next stop Streetsville with time to register, drop our stuff in our rooms and grab a bite before the first clinic. I managed to get in a couple of clinics, deposit my models and photos in the contest room, stay up too late chatting and finally hit the hay.

Breakfast at seven; catch two more clinics and take photos of all the models for the Flimsy (editor's hat on) and then the three amigos are off to see layouts. Luckily many are clustered so as we wend our way through 9 or 10 we are only 25 minutes from the convention by the time the clock has run out for the day. Like all tours the layouts cover the full gamut of the hobby, traditional, innovative, small, large, complete, plywood pacific. The winner of the day was a garden layout complete with yards, multiple routes, buildings and more people and vehicles than I could ever think of having to pick up! A really nice complete package right down to the garden layout around one of the houses. Several of the "in progress layouts" demonstrate the current influences in the hobby; finished train rooms, excellent lighting, backdrops in and complete as possible, fascias, shadowboxing and open lower level staging.

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Some samples from the contest room:

Left: Service Station and automobile repair shop diorama

Bottom Right: Industrial Building kit bashed from Walthers Modular Sections to fit into a triangular space

Bottom Left: Loading dock detail with interior finish

Photos: Peter Gray



Back at the Hotel, we had time for some refreshments before the manufacturers showcase opened and this provided an opportunity to interact with several of the owners and discuss their products, what was in the pipeline, twist arms for your favorite next item, or just shoot the breeze in a relaxed manner.

This was followed by an open panel discussion on the current state of our hobby, where it is going and what is influencing the hobby and the manufacturing and retail environment. The moderator kept things going with some seen questions or request to the panel and then allowed the audience to ask questions or provide input and opinion. The session was quite interesting, entertaining and educational with the proceedings civil and engaging. Bottom line for me is that entrepreneurial ventures are not for the faint of heart!

This session was followed by 'Midnight Madness' a shopping expedition to the Credit Valley Railway Ltd. store, with discounts and door prizes and such. Now by this time it was well past my bed time and I think I wandered around in a daze for most of the time although some items managed to mysteriously appear in my hotel room the next morning (big grin). And I need to write Bob Scott of Credit Valley a nice thank you note as I am not sure I responded appropriately when I was handed a door prize of an ESU LokSound Select Micro decoder and speaker package as a door prize.

Sunday morning came early with the AGM set for seven on the dot. The meeting was mercifully brief (at least for a NFR AGM) and not fraught with controversy so we all survived till the 'Hobo Breakfast' which led us into the awards ceremony for the model and photo contests. Several of our members took home ribbons and such which will be detailed in the June Flimsy.

Models and photos collected, everything packed in the car and we are off for home. One final stop; the York model railroad open house, for a look at this large club layout with lots of impressive models as they recreate southwest Ontario circa 1950 with an emphasis on fidelity.

Our run home is under a glorious summer sun, no hint of trains as we zoom along the 401. A nice way to end a weekend of renewed friendships, and revived interest in our favorite hobby.

## TimeTable

Date	Meetings / Shows	SLD Workshops
May 25, 2013	Bayridge Alliance Church 825 Gardiners Road Kingston, ON	
September 28, 2013	tbd	
October 26, 2013		Emmanuel United Church
November 30, 2013	Emmanuel United Church	Emmanuel United Church
January 25, 2014	Emmanuel United Church	
February 22, 2014		Emmanuel United Church
March 29, 2014	Emmanuel United Church	
April 26, 2014		Emmanuel United Church
May 31, 2014	tbd	

# Train Expo 2013

By Andreas Mank

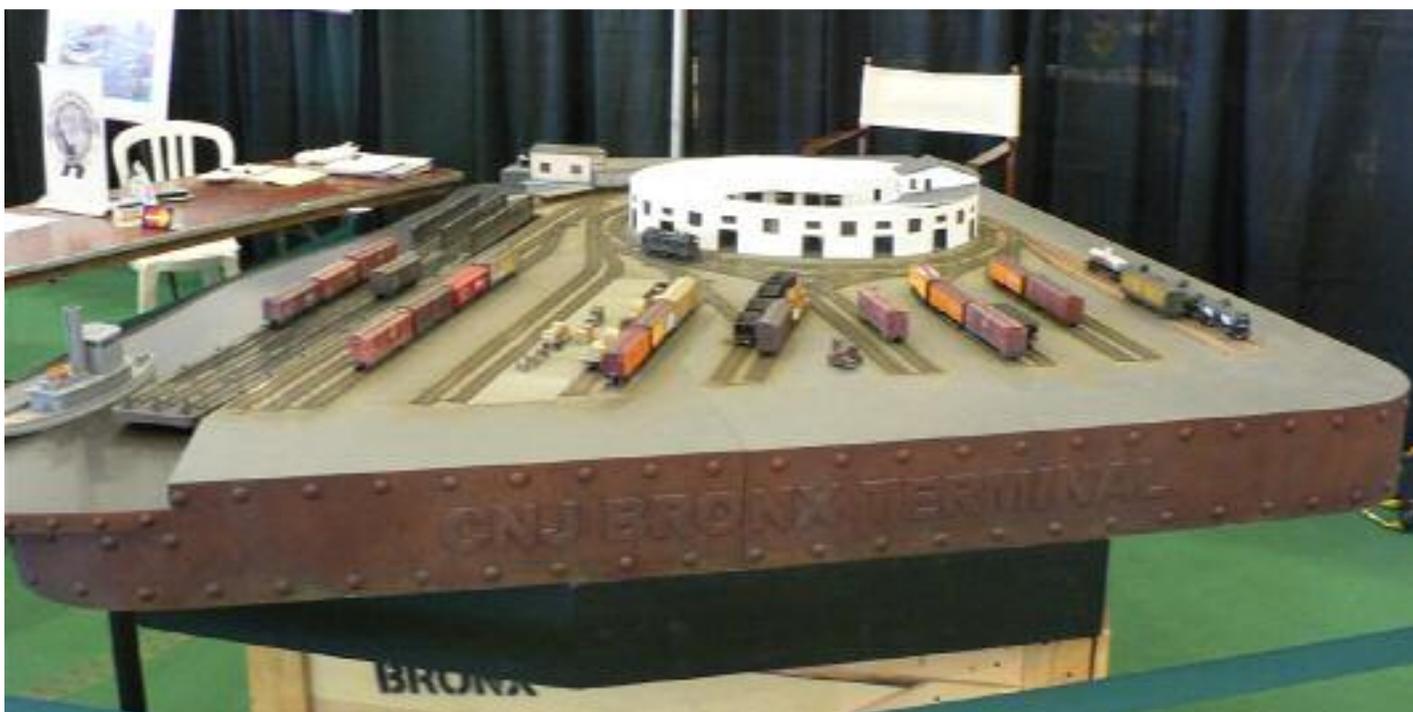
The first weekend in May saw the second edition of Train Expo in Ottawa. As last year with the inaugural show, this year's show consisted of a mix of exhibitions, vendors, clinics and modelling tables. Attendance was respectable considering the early start of summer in Ottawa and slightly higher on Saturday than on Sunday. This year, there were fewer clinics (only one clinic area instead of two) and one corner was set aside as a train themed kids play area.



The mix of vendors and exhibitions also changed, favourites from last year returned and new vendors and groups were added for this year's show. Unfortunately, one popular vendor of craftsman kits from New England was turned away at the border. Several model railway groups and railway enthusiasts were represented, including the SLD. The front cover has a picture that shows our merry modelling table, with the four amigos sporting the new SLD shirts.

Left: Vermont and Essex Railway exhibit layout. The V&E was also present last year, but showed some additions this year

Bottom: CNJ Bronx Terminal. Much progress has been made on this accurate presentation of a very interesting prototype.



I think that the basic concept for the show is good and presume that the organizers will continue to refine the details for next years show. I will close with a few pictures from what I would consider highlights of the show.

Right: This small oval layout is a great example for all armchair modellers of the high quality work that can be done in a small place.

Center: The Brockville and Westport Diorama uses the size of N scale to its advantage by showing the railroad as part of a larger landscape.



Below: The Calabogie, Renfrew and Madawaska Railway returned this year as well. This beautiful Diorama and switching layout displays one of the advantages of working in On30 – sizeable models in a space not larger than that required for HO scale



# Display Table

By Ron Newby

Even though the theme for the March meet was rolling stock, they were few and far between as a lot of other great items were brought out. Stan Conley graciously did the honours of walking us through the models again this month.



First up was the completed large scale model of the Clarke Junction station that Lorne Munro brought out previously while it was under construction and it is impressive. Due to its size and final location different materials and finishes were used that smaller scale model would not normally use. Another item we do not normally do for our inside models is to put a wire screen on the bottom to keep the animals out.

Peter Joyce brought one wall of the St Lambert shops he is building for his transit line. It is being kitbashed from a 2 stall engine house kit. Peter explained that the vines on the walls are used to hide some minor defects and joining lines. The way it looks you never would have known that there were minor defects and joining lines.

John Stewart gave us an excellent clinic on working with metal and one of the items we got to play with was the Unimat lathe that he had brought in. While no longer being made they are still available on Ebay and there are still mom and pop operations that support them.

Next on the display table were two Bachmann 70 tonners brought in by

Gary Baillargeon that were custom painted for CN. Gary also brought in a couple of Hi-railers, one lettered for CN the other for CP.

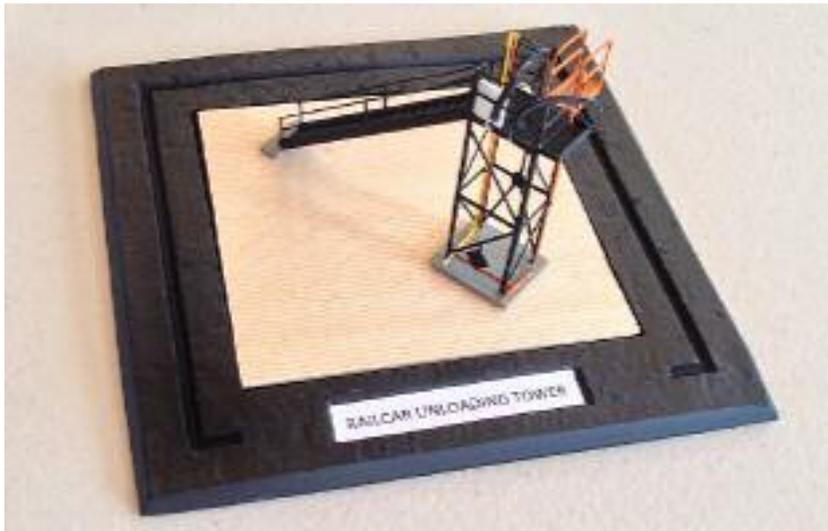
Bill Meredith brought in a station he is scratch building in styrene. He also brought in another building he is scratch building. At this point they are just sides that have been laser cut using a material called rowmark. Rowmark is a tougher and harder to cut than styrene but easily goes together using MEK. Both buildings are in HO scale.

Gilbert Lacroix brought out a completed model of a railcar unloading tower that he is producing as a kit. For his first kit offering it is a beauty.



I brought out a couple of items, the first being a standard Bachmann On30 boxcar that I decaled with custom decals I had made for my railway empire. The second item was a wall plaque of my logo. I found a company at the last Springfield show that made custom plaques. I sent the artwork and a month later I had the plaque. The last item I brought was the completed Dallas Divide Section House that I am using for forced perspective on my layout. No paint or stain was used on the model. All colouring was done using Promarker pens.

Top: Scratchbuilt Station by Bill Meredith  
 Center: Railcar unloading tower by Gilbert LaCroix  
 Bottom: V Hooker & Sons by Dave Primeau



Stan Conley also brought out a couple of items, the first being a completed and painted model of a 1954 Windsor 4 door sedan made by Sylvain Models. He also brought out some fine looking outdoor light made with stainless steel tubing, magnet wire and SMD LEDs. According to Stan it took all of one evening to make the first one but each one after that took less time.



Last but not least Dave Primeau showed some fine structures, the first being his progress on SRMW's V Hooker & Sons. It is an interesting building with interesting lines made with multiples materials. He also brought out a model of an E&N station and the Barry's Bay station which were for sale if anyone wanted them.

That's it for this month, thank you to everyone who brought out their items for us to examine and enjoy.

Additional photos are available on the March meet web page:  
[http://sld-nmra.ca/meets/mar\\_13/mar\\_13.htm](http://sld-nmra.ca/meets/mar_13/mar_13.htm)



# Next Division Meet

St. Lawrence Division – NMRA

**When:**

**Saturday, May 25, 2013**

**Where:**

**Bayridge Alliance Church**  
825 Gardiners Road,  
Kingston

Doors open at 9:30 am -- Admission \$7.00

**What's on:**

**Morning:**

**Division Business**

**Clinics:**

Bob Farquhar

*Building a waterfront layout*

Ron Newby

*Designing a Narrow Gauge  
Layout for Operations*

**Display:**

*Structures*

**Afternoon:**

*Layout Tours*

